

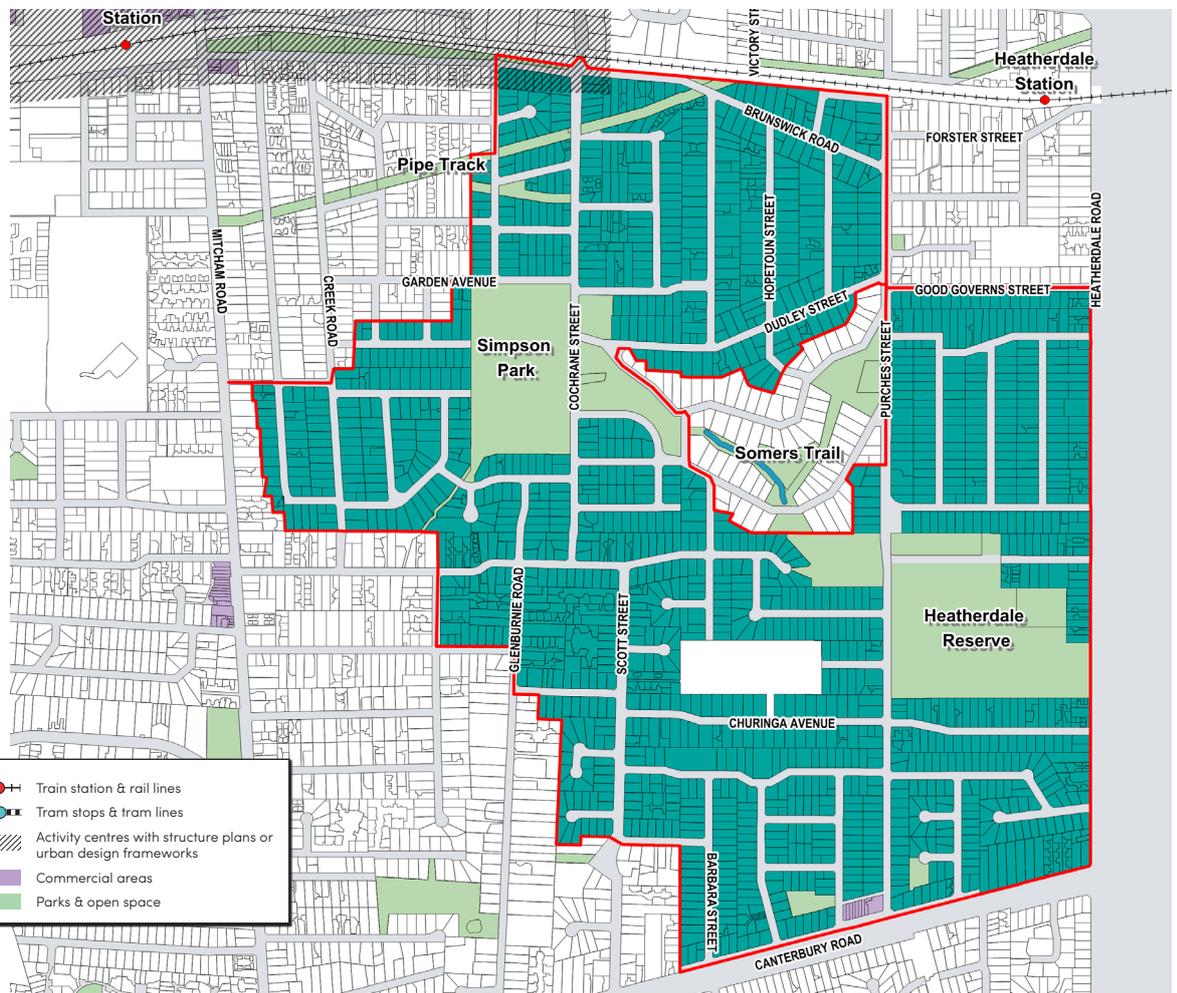
**KEY EXISTING CHARACTERISTICS**

- Architecture is mixed with post-war 1950s and 1960s double or triple fronted dwellings interspersed with 1980s and contemporary infill.
- Materials are generally brick and timber, with tiled roofs.
- Dwellings are predominantly 1-2 storeys and detached with some two storey and semi-detached infill (units and townhouses).
- Front setbacks generally range from 4-8m, with 1-3m and 3-4m side setbacks from both side boundaries. Rear setbacks are usually 5-16m.
- Garages or carports are nonexistent or located behind the front facade, usually along one side boundary with a single crossover.
- Front fences are generally nonexistent or planted with vegetation. There are some very low (up to 0.8m) brick or timber fences allowing views of front gardens, including lawns.
- Gardens are established and generally heavily planted with native and exotic plants, consisting of shrubs, garden beds, lawns and canopy trees.
- Road treatments are sealed with upstanding kerbs and footpaths.
- Street trees are mixed species and formally or informally planted, with some avenue planting.
- The topography of the area is predominantly rolling.
- The precinct abuts and surrounds areas noted and protected for their landscape significance

**PREFERRED CHARACTER STATEMENT**

The dwellings will sit within established bushy garden settings, comprising large canopy trees and extensive native planting. The buildings are often partially hidden by vegetation and, while evident in the streetscape vista, do not dominate the streetscape. Large native and some exotic trees on public and private land provide a backdrop of vegetation, visible between and behind dwellings. Front fences are either not provided or are low, contributing to an informal and vegetation fringed streetscape. Properties abutting and close to the parklands (Heatherdale Reserve, Simpson Park and Melbourne Water pipe reserve) will contain more trees that complement the park landscape and contribute to the overall tree canopy.

Areas around Mitcham station and the MegaMile Activity Centre will accommodate more dwellings with slightly more compact siting than the remaining residential areas, but with space for large trees and gardens.



# BUSH SUBURBAN PRECINCT 4 GUIDELINES

CHARACTER ELEMENT	OBJECTIVE	DESIGN RESPONSE	AVOID
GARDENS & LANDSCAPING	<i>To maintain and strengthen the garden setting of the dwellings and the tree canopy of the neighbourhood.</i>	<ul style="list-style-type: none"> <li>Retain established or mature trees and provide for the planting of new canopy trees and substantial vegetation.</li> <li>Locate footings and paved areas outside the root zone of established trees.</li> <li>Prepare and implement a landscape plan that includes substantial trees and vegetation.</li> <li>Buildings should not exceed 40% site coverage.</li> <li>Provide at least 40% of the site as permeable surface.</li> <li>Provide for two ground level areas with minimum dimensions of 5m x 5m, for open space to accommodate substantial trees.</li> <li>Plant at least two canopy trees with a minimum mature height of 12 metres per dwelling.</li> <li>Open space areas should be oriented to the north wherever possible.</li> </ul>	<p>Removal of large, established trees.</p> <p>Loss of established vegetation.</p> <p>Inadequate space for trees/planting around buildings.</p> <p>Use of an easement or service area for the provision of space for a canopy tree.</p>
	<i>To minimise the loss of front garden space and the dominance of car parking structures.</i>	<ul style="list-style-type: none"> <li>Provide only one vehicular crossover per typical site frontage.</li> </ul>	<p>Car parking structures that dominate the façade or view of the dwelling from the street.</p> <p>Creation of new crossovers and driveways, or wide crossovers.</p> <p>Excessive areas of hard paving and driveways.</p>
	<i>To maintain the sense of openness and visibility of tree canopies in rear gardens.</i>	<ul style="list-style-type: none"> <li>Buildings should not exceed the dominant tree canopy height.</li> <li>Landscape plans for new developments should include canopy trees in rear gardens.</li> </ul>	<p>Bulky development dominating the tree canopy.</p> <p>Lack of space for large trees.</p>
	<i>To ensure the provision of permeable and useable private open space for new dwellings.</i>	<ul style="list-style-type: none"> <li>In addition to any new balconies or rooftops, private open space with a minimum dimension of 5m x 5m for each dwelling should be provided.</li> <li>Private open space should be oriented to the north wherever possible and accommodate garden planting.</li> </ul>	<p>Inadequate permeable private open space.</p>
	<i>To ensure the spacing and density of dwellings is managed to accord with housing objectives.</i>	<ul style="list-style-type: none"> <li>The minimum subdivision area should be 320 sq. m.</li> <li>A permit is required for the construction or extension of one dwelling on a lot that is less than 500 sq. m. Development of single dwellings on lots smaller than 500 sq. m. should only be approved if all other guidelines are satisfied.</li> </ul>	<p>Lot sizes and development that does not meet the other neighbourhood character Guidelines.</p>
MINIMUM LOT SIZE	<i>To ensure the consistency, where present, of front boundary setbacks.</i>	<ul style="list-style-type: none"> <li>New buildings should be setback to reflect the prevailing front setbacks.</li> </ul>	<p>Loss of front garden space and / or consistent front setback patterns.</p>
	<i>To maintain and reinforce the rhythm of spacing between and around buildings, and the alignment of buildings along the street.</i>	<ul style="list-style-type: none"> <li>Set back buildings a minimum of 1m from one side boundary, and a minimum of 3m from the other side boundary.</li> <li>Setback buildings a minimum of 5m from the rear boundary.</li> <li>Provide a separation of at least 3-4 metres between dwellings on the same site to accommodate vegetation.</li> <li>Carports, garages or outbuildings should be setback a minimum of 1m from the front façade of a dwelling fronting the street, with no walls on a boundary.</li> </ul>	<p>Inconsistent siting patterns and a lack of space around buildings.</p>
SENSITIVE LANDSCAPE ENVIRONS	<i>To ensure buildings make a positive contribution to adjacent creeklands and bushland reservations and areas of environmental or landscape significance, and other sensitive landscape environs.</i>	<ul style="list-style-type: none"> <li>Minimise the visual impact of development on the adjacent sensitive landscape area.</li> <li>Building design should respond to the topography and minimise the need for cut and fill.</li> <li>Provide landscape plans that incorporate substantial use of indigenous and native trees and vegetation to reflect that in the adjacent sensitive landscape area.</li> <li>Minimise site coverage and hard surfaces on sites adjoining sensitive landscape environs.</li> </ul>	<p>Buildings that are visually dominant when viewed from within the sensitive landscape.</p> <p>Buildings that do not respond to the topography.</p> <p>Excessive site coverage and hard surfaces that leave inadequate space for trees and vegetation to complement the sensitive landscape environs.</p>
	<i>To ensure that new development provides a positive interface with any adjoining parks and open space.</i>	<ul style="list-style-type: none"> <li>Design new buildings to provide a façade to any adjacent parkland to enable casual passive surveillance of the public space.</li> <li>Buildings should be setback from the interface boundary so as not to dominate or appear overbearing from within the parkland.</li> </ul>	<p>Blank walls fronting parkland space.</p>
PARK INTERFACE	<i>To ensure that buildings and extensions do not dominate the streetscape.</i>	<ul style="list-style-type: none"> <li>Buildings should not exceed two storeys (8m) in height.</li> <li>Roof forms should incorporate eaves.</li> </ul>	<p>Buildings that appear to exceed the predominant height of buildings in the street by more than one storey.</p> <p>Lack of eaves.</p>
	<i>To encourage building detailing that reflects, without mimicking, the details of buildings in the area.</i>	<ul style="list-style-type: none"> <li>Articulate the facades of buildings with the use of recesses, verandahs, balconies, window openings and variations in materials and colours.</li> </ul>	<p>Blank walls and facades.</p> <p>Mock historical styles and 'reproduction' detailing.</p>
BUILDING HEIGHT & FORM	<i>To encourage building detailing that reflects, without mimicking, the details of buildings in the area.</i>	<ul style="list-style-type: none"> <li>Articulate the facades of buildings with the use of recesses, verandahs, balconies, window openings and variations in materials and colours.</li> </ul>	<p>Blank walls and facades.</p> <p>Mock historical styles and 'reproduction' detailing.</p>
MATERIALS & DESIGN DETAIL	<i>To retain views to dwellings and gardens, and complement the predominant style of front boundary delineation in the street.</i>	<ul style="list-style-type: none"> <li>No front fence or a low or open style front fence up to 1m height. Utilise vegetation as an alternative to fencing where possible.</li> <li>On main roads, higher front fences (up to 1.8m) may be constructed where they provide at least 50% permeability.</li> </ul>	<p>High, solid front fencing.</p>
	<i>To retain views to dwellings and gardens, and complement the predominant style of front boundary delineation in the street.</i>	<ul style="list-style-type: none"> <li>No front fence or a low or open style front fence up to 1m height. Utilise vegetation as an alternative to fencing where possible.</li> <li>On main roads, higher front fences (up to 1.8m) may be constructed where they provide at least 50% permeability.</li> </ul>	<p>High, solid front fencing.</p>
FRONT FENCING	<i>To retain views to dwellings and gardens, and complement the predominant style of front boundary delineation in the street.</i>	<ul style="list-style-type: none"> <li>No front fence or a low or open style front fence up to 1m height. Utilise vegetation as an alternative to fencing where possible.</li> <li>On main roads, higher front fences (up to 1.8m) may be constructed where they provide at least 50% permeability.</li> </ul>	<p>High, solid front fencing.</p>