

## KEY EXISTING CHARACTERISTICS

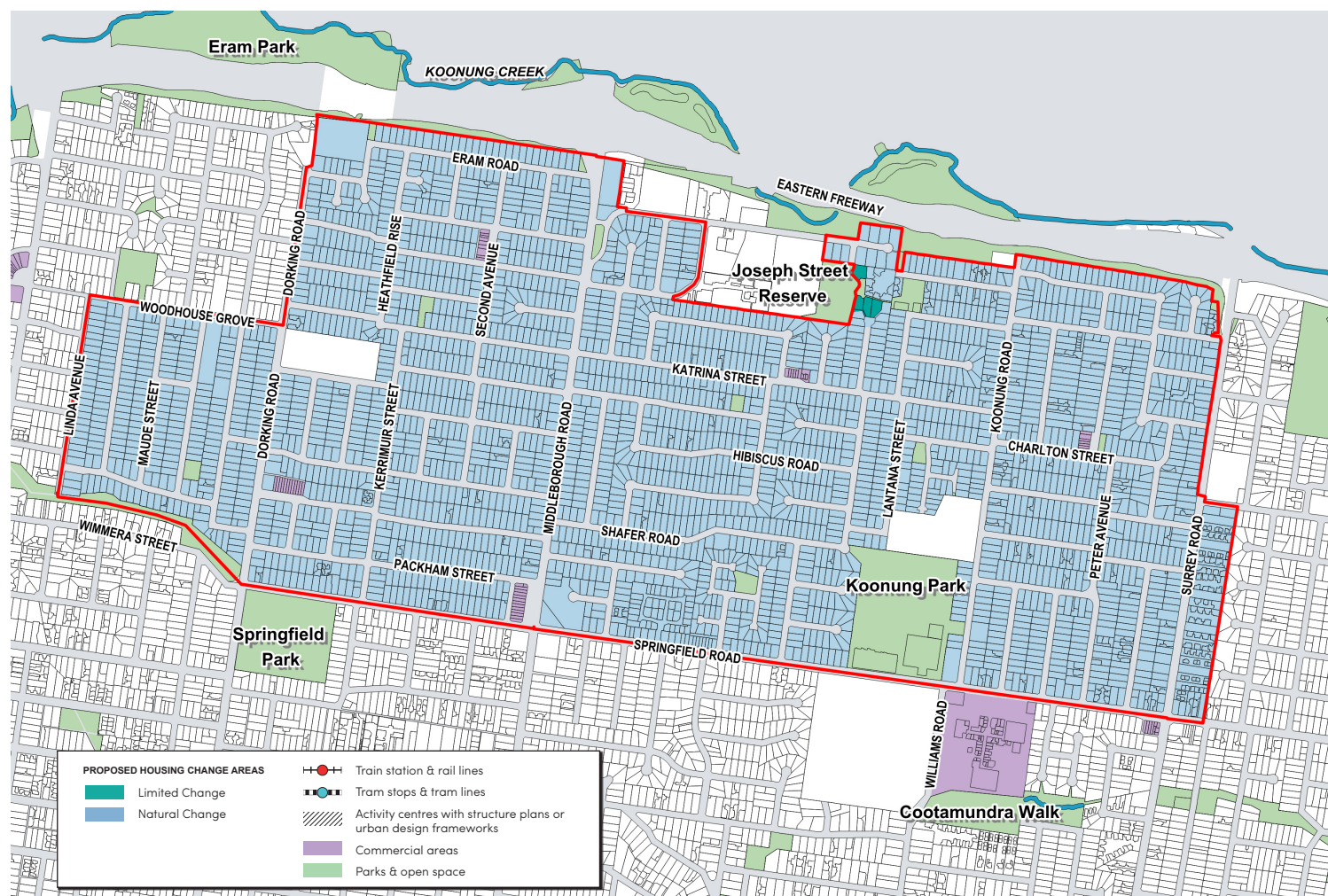
- Architecture is predominantly post-war to 1960s cream brick and weatherboard homes, with a significant number of 1980s to contemporary infill.
- Building materials are predominantly brick or timber weatherboard, with tiled roofs and some render infill.
- Front setbacks are usually 5-8m, with 1-3m side setbacks usually from both side boundaries. Some new developments have smaller front (3-5m) and side setbacks (0-1m).
- Dwellings are 1-2 storeys, detached and semi-detached (units).
- Garages and carports are usually located to the rear of dwellings, along the side boundary and with a single crossover.
- Front fences are nonexistent, planted with vegetation or low in height (up to 1.2m), and usually constructed of brick or timber.
- Garden styles are generally low and established with exotic vegetation, consisting of shrubs, garden beds, lawns and canopy trees.
- Roads are sealed with upstanding kerbs with footpaths on both sides of the street.
- Street trees are regularly planted with mixed species and sizes.
- The topography of the area is predominantly rolling with gentle slopes, and some flat areas.

## PREFERRED CHARACTER STATEMENT

The area will retain its classic garden suburban characteristics of low set, pitched roof dwellings set in spacious garden settings, with a backdrop of large native and exotic trees. The established pattern of regular front and side setbacks from both side boundaries will be maintained, allowing sufficient space for planting and growth of new vegetation.

Infill development including unit developments will be common, however new buildings and additions will be setback at upper levels to minimise dominance in the streetscape. Low or open style front fences will provide a sense of openness along the streetscape, and allow views into front gardens and lawn areas.

Buildings close to Koonung Creek and Bushy Creek parklands will be sited so that the overall visibility of the development is minimised when viewed from these areas, which will enhance their natural, bushy settings. Vegetation from private gardens will enhance the existing landscape character of the creek corridor or parklands, incorporating large native / indigenous canopy trees.



# GARDEN SUBURBAN PRECINCT 9 GUIDELINES

CHARACTER ELEMENT	OBJECTIVE	DESIGN RESPONSE	AVOID
GARDENS & LANDSCAPING	<i>To maintain and strengthen the garden setting of the dwellings and the tree canopy of the neighbourhood.</i>	<ul style="list-style-type: none"> <li>Retain established or mature trees and provide for the planting of new canopy trees and substantial vegetation.</li> <li>Locate footings and paved areas outside the root zone of established trees</li> <li>Prepare and implement a landscape plan that includes substantial trees and vegetation.</li> <li>Site coverage should not exceed 50%.</li> <li>Provide at least 30% of the site as permeable surface.</li> <li>Provide for one ground level area with minimum dimensions of 5m x 5m, for open space to accommodate at least one canopy tree.</li> <li>Plant at least two canopy trees with a minimum mature height of 8 metres per dwelling.</li> <li>Open space areas should be oriented to the north wherever possible.</li> </ul>	<p>Removal of large, established trees.</p> <p>Loss of established vegetation.</p> <p>Inadequate space for trees/planting around buildings.</p> <p>Use of an easement or service area for the provision of space for a canopy tree.</p>
	<i>To minimise the loss of front garden space and the dominance of car parking structures.</i>	<ul style="list-style-type: none"> <li>Provide only one vehicular crossover per typical site frontage.</li> </ul>	<p>Car parking structures that dominate the façade or view of the dwelling from the street.</p> <p>Creation of new crossovers and driveways, or wide crossovers.</p> <p>Excessive areas of hard paving and driveways.</p>
	<i>To ensure the provision of permeable and useable private open space for new dwellings.</i>	<ul style="list-style-type: none"> <li>In addition to any new balconies or rooftops, private open space with a minimum dimension of 5m x 5m for each dwelling should be provided.</li> <li>Private open space should be oriented to the north wherever possible and accommodate garden planting.</li> </ul>	Inadequate permeable private open space.
MINIMUM LOT SIZE	<i>To ensure the spacing and density of dwellings is managed to accord with housing objectives.</i>	<ul style="list-style-type: none"> <li>In Limited Change areas / sites, the minimum subdivision area should be 320 sq. m.</li> <li>A permit is required for the construction or extension of one dwelling on a lot that is less than 300 sq. m. Development of single dwellings on lots smaller than 300 sq. m. should only be approved if all other guidelines are satisfied.</li> </ul>	Lot sizes and development that does not meet the other neighbourhood character Guidelines.
SITING	<i>To maintain and reinforce the rhythm of spacing between and around buildings, and the alignment of buildings along the street</i>	<ul style="list-style-type: none"> <li>New buildings should be setback to reflect the prevailing front setbacks.</li> <li>Set back buildings a minimum of 1m from at least one side boundary in Limited Change areas.</li> <li>Set back buildings a minimum of 1m from both side boundaries in Natural Change areas.</li> <li>In Limited Change areas carports, garages or outbuildings may be located on one side boundary where it is setback a minimum of 1m from the front facade of a dwelling fronting the street.</li> <li>In Natural Change areas carports, garages or outbuildings should be setback a minimum of 1m from the front façade of a dwelling fronting the street, with no walls on a boundary.</li> </ul>	Inconsistent siting patterns and a lack of space around buildings.
SENSITIVE LANDSCAPE ENVIRONS	<i>To ensure buildings make a positive contribution to adjacent Koonung Creek reservation and Bushy Creek Parklands.</i>	<ul style="list-style-type: none"> <li>Minimise the visual impact of development on the adjacent sensitive landscape area.</li> <li>Provide landscape plans that incorporate substantial use of indigenous and native trees and vegetation to reflect that in the adjacent sensitive landscape area.</li> <li>Minimise site coverage and hard surfaces on sites adjoining sensitive landscape environs.</li> </ul>	<p>Buildings that are visually dominant when viewed from within the sensitive landscape.</p> <p>Buildings that do not respond to the topography.</p> <p>Excessive site coverage and hard surfaces that leave inadequate space for trees and vegetation to complement the sensitive landscape environs.</p>
PARK INTERFACE	<i>To ensure that new development provides a positive interface with any adjoining parks or open space.</i>	<ul style="list-style-type: none"> <li>Design new buildings to provide a façade to any adjacent parkland to enable casual passive surveillance of the public space.</li> <li>Building should be setback from the interface boundary so as not to dominate or appear overbearing from within the parkland.</li> </ul>	Blank walls fronting parkland spaces.
BUILDING HEIGHT & FORM	<i>To ensure that buildings and extensions do not dominate the streetscape.</i>	<ul style="list-style-type: none"> <li>Buildings should not exceed two storeys in height.</li> <li>Roof forms should incorporate eaves.</li> </ul>	<p>Buildings that appear to exceed the predominant height of buildings in the street by more than one storey.</p> <p>Lack of eaves.</p>
MATERIALS & DESIGN DETAIL	<i>To encourage building detailing that reflects, without mimicking, the details of buildings in the area.</i>	<ul style="list-style-type: none"> <li>Articulate the facades of buildings with the use of recesses, verandahs, balconies, window openings and variations in materials and colours.</li> </ul>	<p>Blank walls and facades.</p> <p>Mock historical styles and 'reproduction' detailing.</p>
FRONT FENCING	<i>To retain views to dwellings and gardens, and complement the predominant style of front boundary delineation in the street.</i>	<ul style="list-style-type: none"> <li>Provide a low or open style front fence up to 1.2m in height, and constructed of materials appropriate to the dwelling style.</li> <li>On main roads, higher front fences (up to 1.8m) may be constructed where they provide at least 20% permeability.</li> </ul>	High, solid front fencing.