

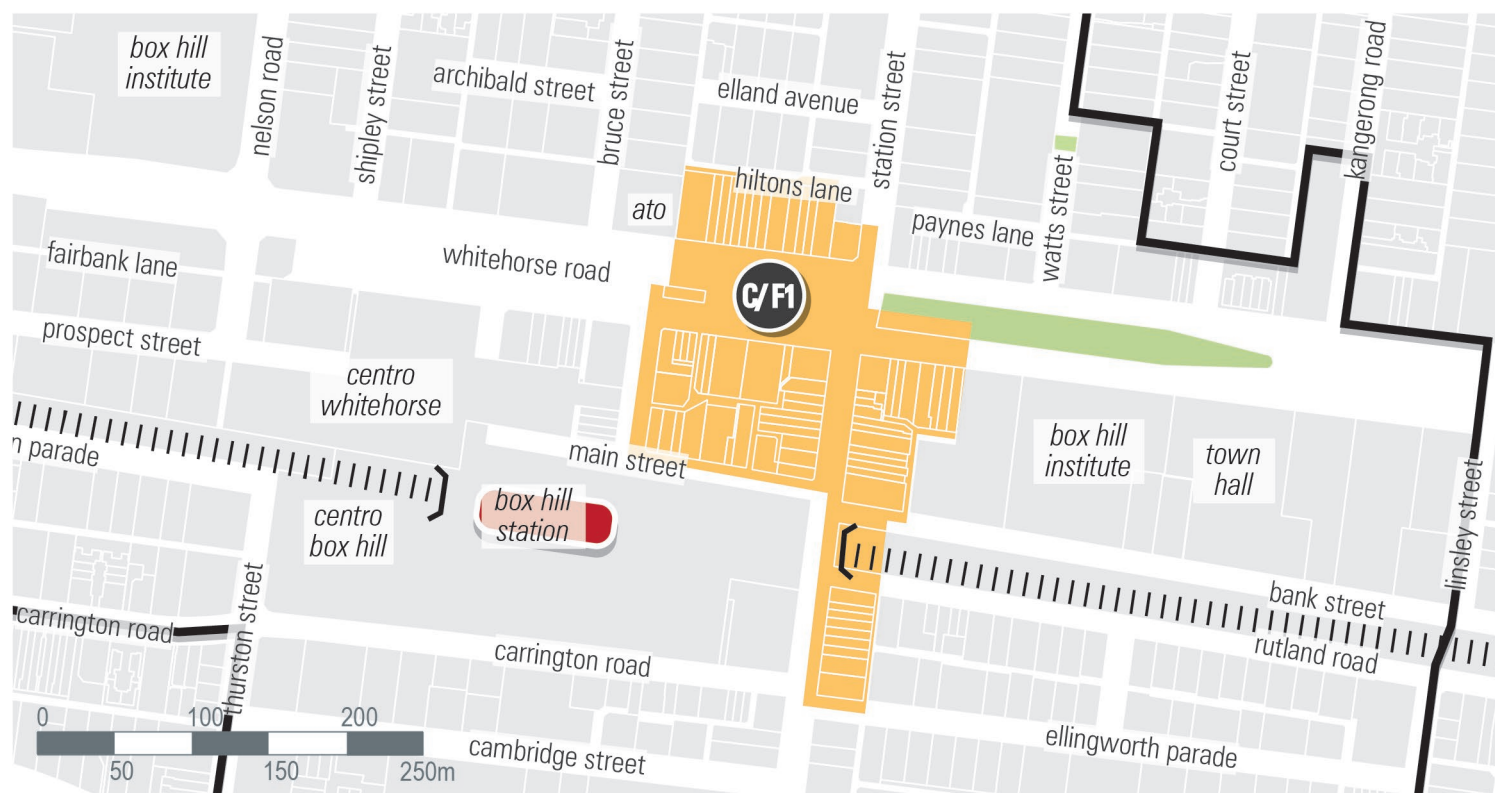
**SUB PRECINCT BUILT FORM GUIDELINES**

**05**

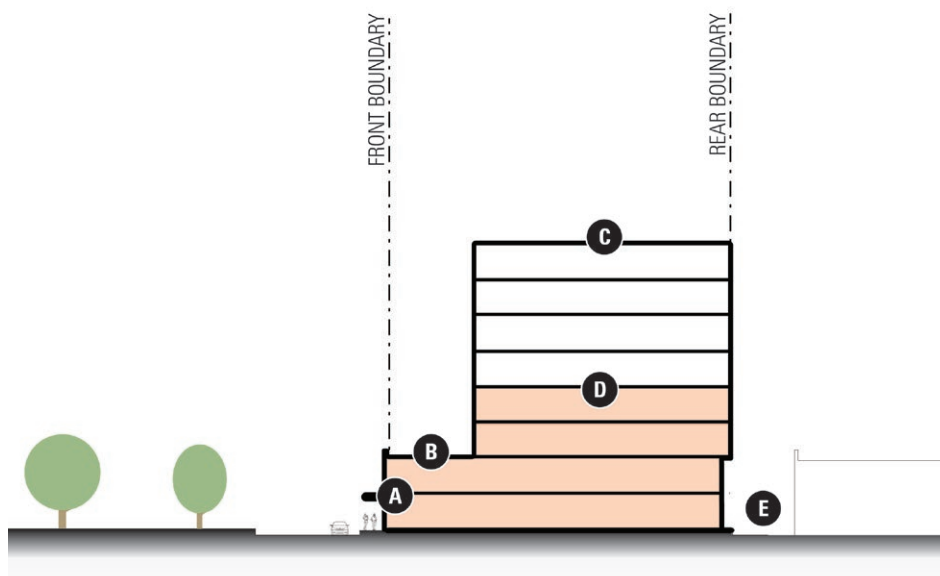
# Sub Precinct Guidelines

## Precinct C/ F1: Traditional Town Centre

**C/F1**



- A** traditional street wall
- B** 10m setback from front boundary
- C** preferred max. building height 8 storey
- D** 3 storey base definition
- E** 1.5m setback from laneway for vehicular access as required



# Sub Precinct Guidelines

## Precinct C/ F1: Traditional Town Centre



**C/F1**

UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>Typical allotment size: Extra Small (less than 700sqm)</li> <li>Typical allotment width: Small (less than 10m)</li> </ul>	<ul style="list-style-type: none"> <li>To discourage site consolidation</li> <li>To support infill re-development above, or behind existing heritage form</li> </ul>	<ul style="list-style-type: none"> <li>100% Site coverage</li> <li>No side setback</li> <li>Party wall arrangement along common boundaries</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Whitehorse Road (60m)</li> <li>Station Street (20m)</li> </ul> </li> <li>Existing traditional street wall of 1-2 storey</li> </ul>	<ul style="list-style-type: none"> <li>To establish a pedestrian scale urban environment</li> <li>To retain a sense of openness to the sky</li> <li>To ensure proportionate relationship between the low scale street wall and potential future addition above/ to the rear</li> </ul>	<ul style="list-style-type: none"> <li>1-2 storey street wall along Whitehorse Road and Station Street</li> <li>Minimum 10m setback for additional levels, measured from primary street boundaries</li> <li>Maximum building height of 8 storey</li> <li>A distinction to the building 'base' design of up to 4 storey to the rear</li> <li>No side setbacks are required</li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>Former Colonial Gas Association Building (942-946 Whitehorse Road)- H091</li> <li>Former Railway Hotel (950-956 Whitehorse Road)- H092</li> <li>Shops (958-964 Whitehorse Road)- H093</li> <li>Shop (953 Whitehorse Road)- amendment H0244</li> <li>Shops - (920-940, 948 &amp; 966-984 Whitehorse Road) - contributory buildings within the Commercial Heritage Precinct (H0244)</li> </ul>	<ul style="list-style-type: none"> <li>To ensure consistency of low scale street wall presentation along Whitehorse Road</li> <li>To recognise the presence of individual significant heritage building</li> </ul>	
<b>Key Views</b>	<ul style="list-style-type: none"> <li>Existing individual significant heritage forms:               <ul style="list-style-type: none"> <li>The former Colonial Gas Association Building (942-946 Whitehorse Road)</li> <li>The former Railway Hotel (950-956 Whitehorse Road)</li> <li>Hanbit Uniting Church (909 Whitehorse Road)</li> <li>953 Whitehorse Road</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>To frame viewlines to heritage forms along Whitehorse Road and Station Street</li> <li>To maintain a sense of openness around the listed heritage buildings in the streetscape</li> </ul>	
<b>Additional street/ laneway address</b>	<ul style="list-style-type: none"> <li>All sites have access to multiple street frontages, or side/ rear laneways</li> <li>Laneways are generally consistently 3- 3.5m wide</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Street Frontages</li> </ul>	<ul style="list-style-type: none"> <li>Setback ground level and level 1 from rear boundary by 1.5m to facilitate vehicular/ service access from rear laneways as required</li> <li>Encourage minimising car parking requirement</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>The central median of Whitehorse Road (between Nelson Road and Graham Place) is an identified Key Open Space</li> <li>The existing 20- storey ATO building (913 Whitehorse Road) has resulted in some overshadowing of Market Street</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> <li>To avoid additional overshadowing of key open space and pedestrian plaza (as shown on page 24)</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>No landscaping provision has been accommodated within private properties</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives- Landscaping</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines- Landscaping</li> </ul>

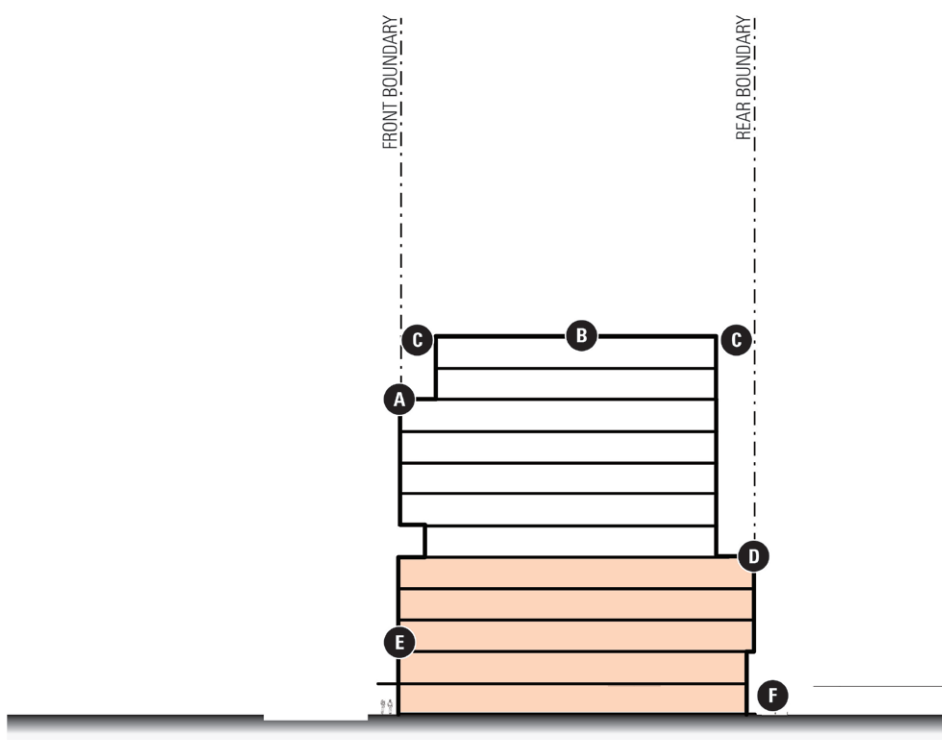
# Building Envelope Guidelines

## Precinct F2: Station Street

**F2**



- A** 10 storey street wall to street frontage
- B** preferred max. building height 12 - 15 storey
- C** 5m setback above street wall
- D** 5 storey street wall to rear
- E** 5 storey base definition
- F** 1.5m setback from laneway for vehicular access as required



# Building Envelope Guidelines

## Precinct F2: Station Street


**F2**

UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>Typical allotment size: Medium (up to 1,500sqm) to Large (up to 2,500sqm)</li> <li>Centro Box Hill site is categorised as Extra Large site (greater than 2,500sqm)</li> <li>Typical allotment width: Medium (15-20m) and Large (20-25m)</li> </ul>	<ul style="list-style-type: none"> <li>To support medium to high density infill development</li> <li>To encourage lot consolidation to support development potential and minimise offsite impacts</li> </ul>	<ul style="list-style-type: none"> <li>100% Site coverage</li> <li>Additional building height is possible on sites which are greater than 1,500sqm subject to positive contribution to its local context</li> <li>A plot - ratio approach to support greater development scale is applicable on Strategic Development Site (to be determined) subject to positive contribution to its local context</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Station Street (20m)</li> <li>Cambridge Street (18m)</li> </ul> </li> <li>Recent infill development of up to 9-10 storey with 4-5 storey street wall along Station Street north (under construction)</li> <li>Approval for 30+ storey mixed use development at 545 Whitehorse Road (not constructed)</li> </ul>	<ul style="list-style-type: none"> <li>To establish a pedestrian scale urban environment</li> <li>To establish a distinctive and consistent urban presence at the entry points of Box Hill from the north and south along Station Street</li> <li>To establish a consistent height datum of robust infill street wall with recessive upper level/s along Station Street south of Whitehorse Road</li> <li>To support high density transit oriented development opportunity at the Centro Box Hill site</li> </ul>	<ul style="list-style-type: none"> <li>10 storey street wall to achieve a maximum 2:1 (street wall to street width) ratio along Station Street south of Whitehorse Road</li> <li>5 storey (20m) street wall to achieve a maximum 1:1 (street wall to street width) ratio along Station Street north of Whitehorse Road</li> <li>Preferred setback of 5m above the street wall</li> <li>Preferred maximum height of 12 storey</li> <li>Preferred maximum height of 15 storey on sites greater than 1,500sqm</li> <li>Centro Box Hill site as a Strategic Development Site</li> <li>No setback from side and rear boundaries for up to 10 storey</li> <li>Minimum 5m setback from side and rear boundaries for 10-15 storey</li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Key Views</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Additional street/ laneway address</b>	<ul style="list-style-type: none"> <li>All sites have access to multiple street frontages, or side/ rear laneways</li> <li>Laneways are generally consistently 3-3.5m wide</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Street Frontages</li> </ul>	<ul style="list-style-type: none"> <li>Setback ground level and level 1 from rear boundary by 1.5m to facilitate vehicular/ service access from rear laneways as required</li> <li>Encourage minimising car parking requirement</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>Key Open Spaces:               <ul style="list-style-type: none"> <li>The central median of Whitehorse Road (between Nelson Road and Graham Place)</li> <li>Carrington Road (between Thurston Street and Station Street)</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>No landscaping provision has been accommodated within private properties</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Landscaping</li> </ul>	<ul style="list-style-type: none"> <li>Encourage establishment of green walls, or landscape elements within the building facade to benefit from northern orientation</li> <li>Establish landscaped roof top gardens on Strategic Development Site</li> </ul>



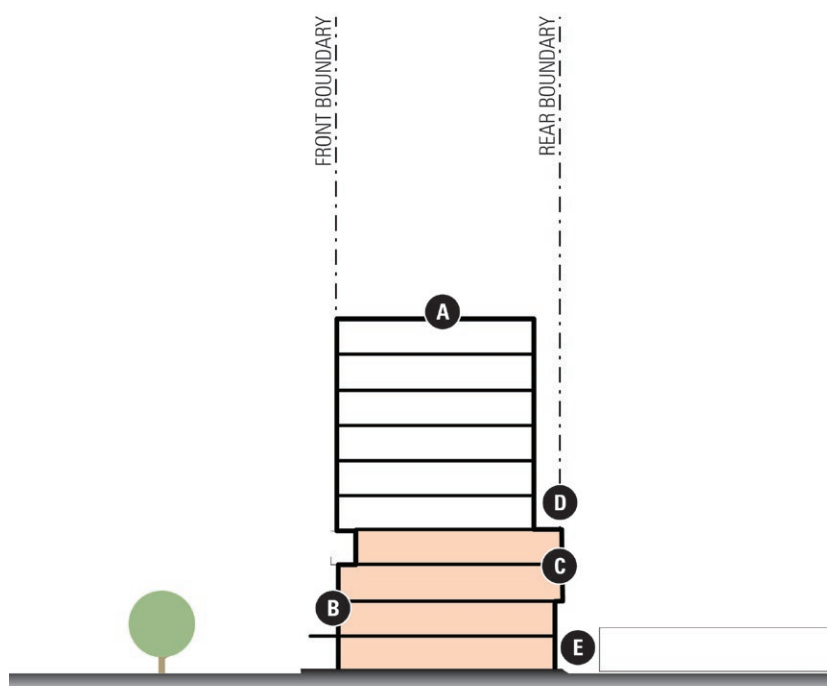
# Building Envelope Guidelines

## Precinct F3: Rutland Road, Watts Street & Carrington Road

F3



- A** preferred max. building height 10 - 12 storey
- B** 4 storey base definition to the street frontage
- C** 4 storey street wall to the rear
- D** 5m setback above street wall to the rear
- E** 1.5m setback from laneway for vehicular access as required



# Building Envelope Guidelines

## Precinct F3: Rutland Road, Watts Street & Carrington Road

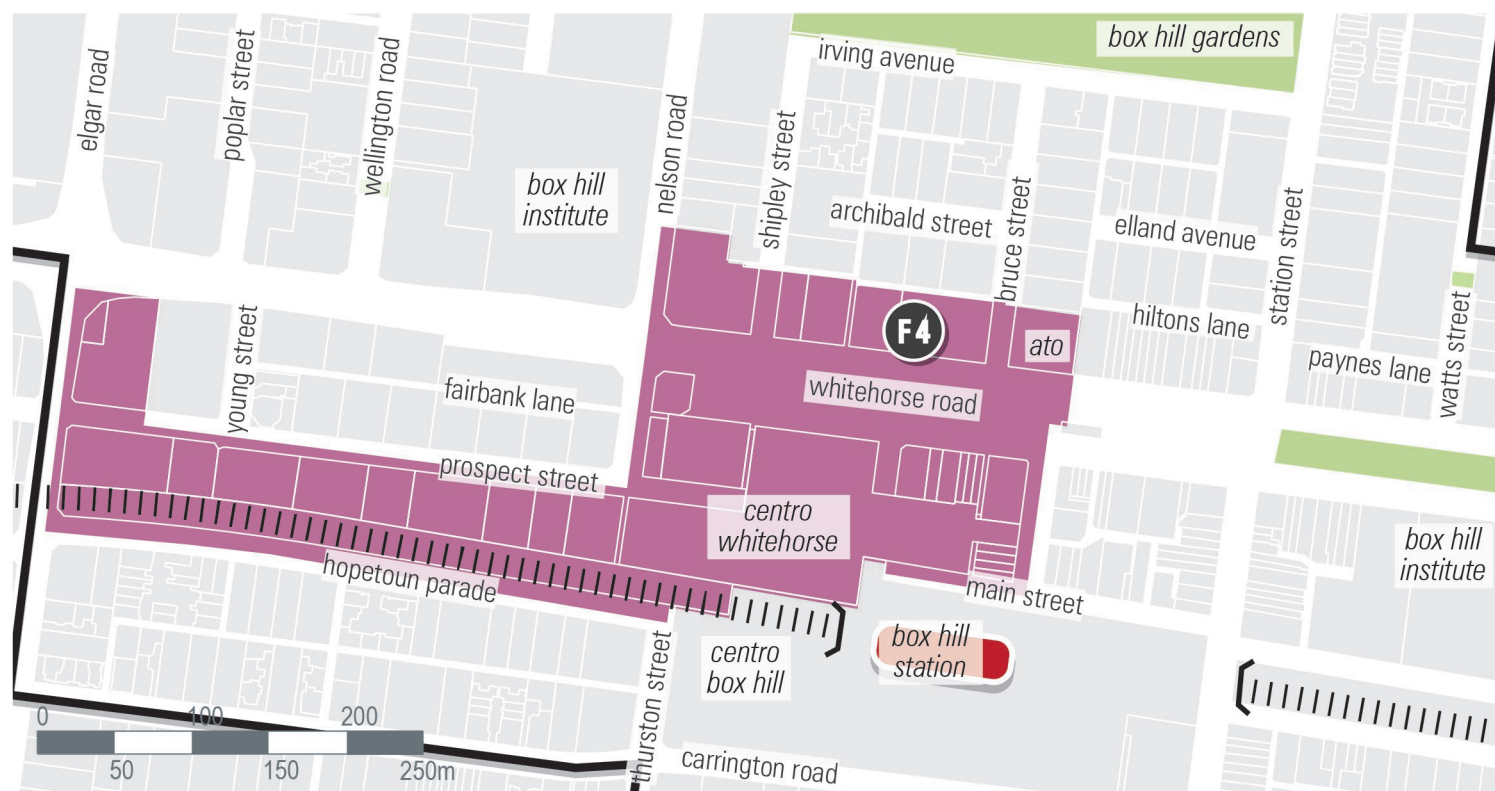

**F3**

UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>Typical allotment size: Small (up to 700sqm) to Medium (up to 1,000sqm)</li> <li>Typical allotment width: Medium (10-20m)</li> </ul>	<ul style="list-style-type: none"> <li>To support medium to high density infill development</li> <li>To encourage lot consolidation to support development potential and minimise offsite impacts</li> </ul>	<ul style="list-style-type: none"> <li>100% Site coverage</li> <li>Additional building heights is possible on sites which are greater than 1,500sqm subject to positive contribution to its local context</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Whitehorse Road (60m)</li> <li>Rutland Road (13.5m)</li> <li>Ellingworth Parade (15m)</li> <li>Carrington Road (15m)</li> </ul> </li> <li>Residential Interface:               <ul style="list-style-type: none"> <li>Thurston Street</li> <li>Linsley Street</li> </ul> </li> <li>Approval for 10+ storey development at 9-11 Ellingworth Parade (not constructed)</li> </ul>	<ul style="list-style-type: none"> <li>To establish a pedestrian scale urban environment</li> <li>To establish a consistent height datum of robust infill street wall with recessive upper level/s</li> <li>To establish a sense of transition on sites with a direct residential interface (outside the Activity Centre)</li> <li>To establish active frontages along Carrington Street, Rutland Road and Ellingworth Parade</li> </ul>	<ul style="list-style-type: none"> <li>10 storey street wall to achieve a maximum 2:1 (street wall to street width) ratio along local streets with a clear distinction to the building base (up to 5 storey)</li> <li>4 storey street walls along residential interface</li> <li>Preferred maximum height of 10 storey</li> <li>Preferred maximum height of 12 storey on sites greater than 1,500sqm</li> <li>Preferred setback of 5m above the street wall</li> <li>No setback from side and rear boundaries for up to 10 storey</li> <li>Minimum 5m setback from side and rear boundaries for 10-12 storey</li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Key Views</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Additional street/ laneway address</b>	<ul style="list-style-type: none"> <li>All sites have access to multiple street frontages, or side/ rear laneways</li> <li>Laneways are generally consistently 3- 3.5m wide</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Street Frontages</li> </ul>	<ul style="list-style-type: none"> <li>Setback ground level and level 1 from rear boundary by 1.5m to facilitate vehicular/ service access from rear laneways as required</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>Key Open Spaces:               <ul style="list-style-type: none"> <li>The central median of Whitehorse Road (between Nelson Road and Graham Place)</li> <li>Carrington Road (between Thurston Street and Station Street)</li> <li>Council car park (22 Ellingworth Parade)</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>No landscaping provision has been accommodated within private properties</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives- Landscaping</li> </ul>	<ul style="list-style-type: none"> <li>Encourage establishment of green walls, or landscape elements within the building facade to benefit from northern orientation</li> <li>Establish a sense of address onto areas identified as key open space</li> <li>Improve urban presence along Rutland Road (railway line) and Carrington Road.</li> </ul>

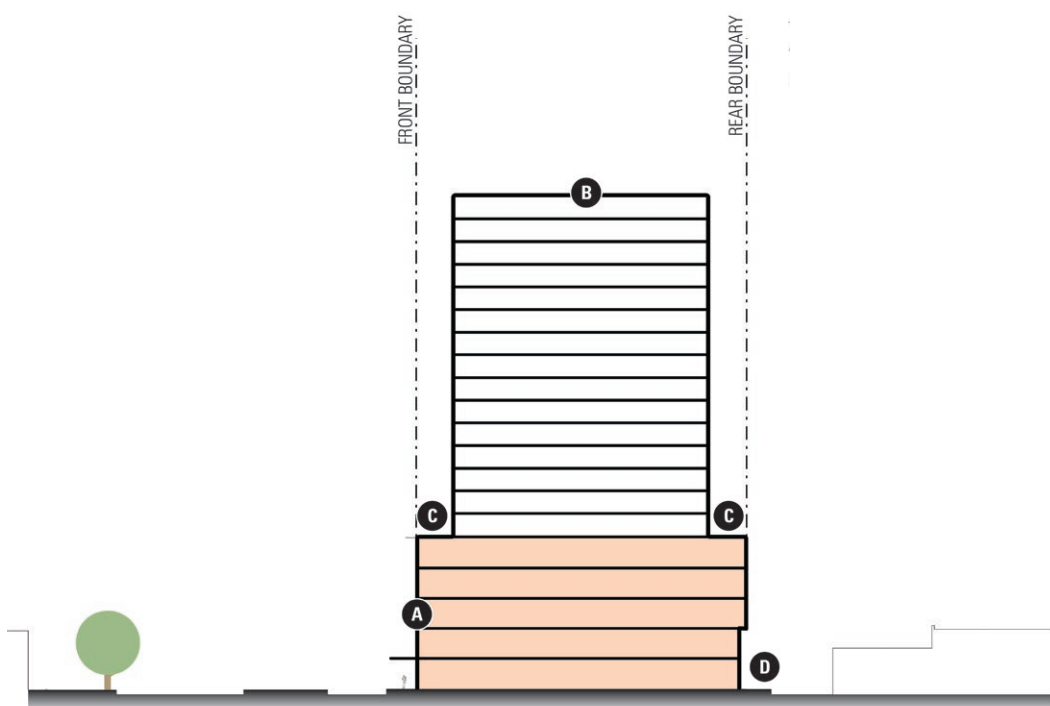
# Building Envelope Guidelines

## Precinct F4: Whitehorse Road & Prospect Street

F4



- A** 5 storey podium
- B** preferred max. building height 20 storey
- C** 5m setback above podium
- D** 1.5m setback from laneway for vehicular access as required





# Building Envelope Guidelines

## Precinct F4: Whitehorse Road & Prospect Street

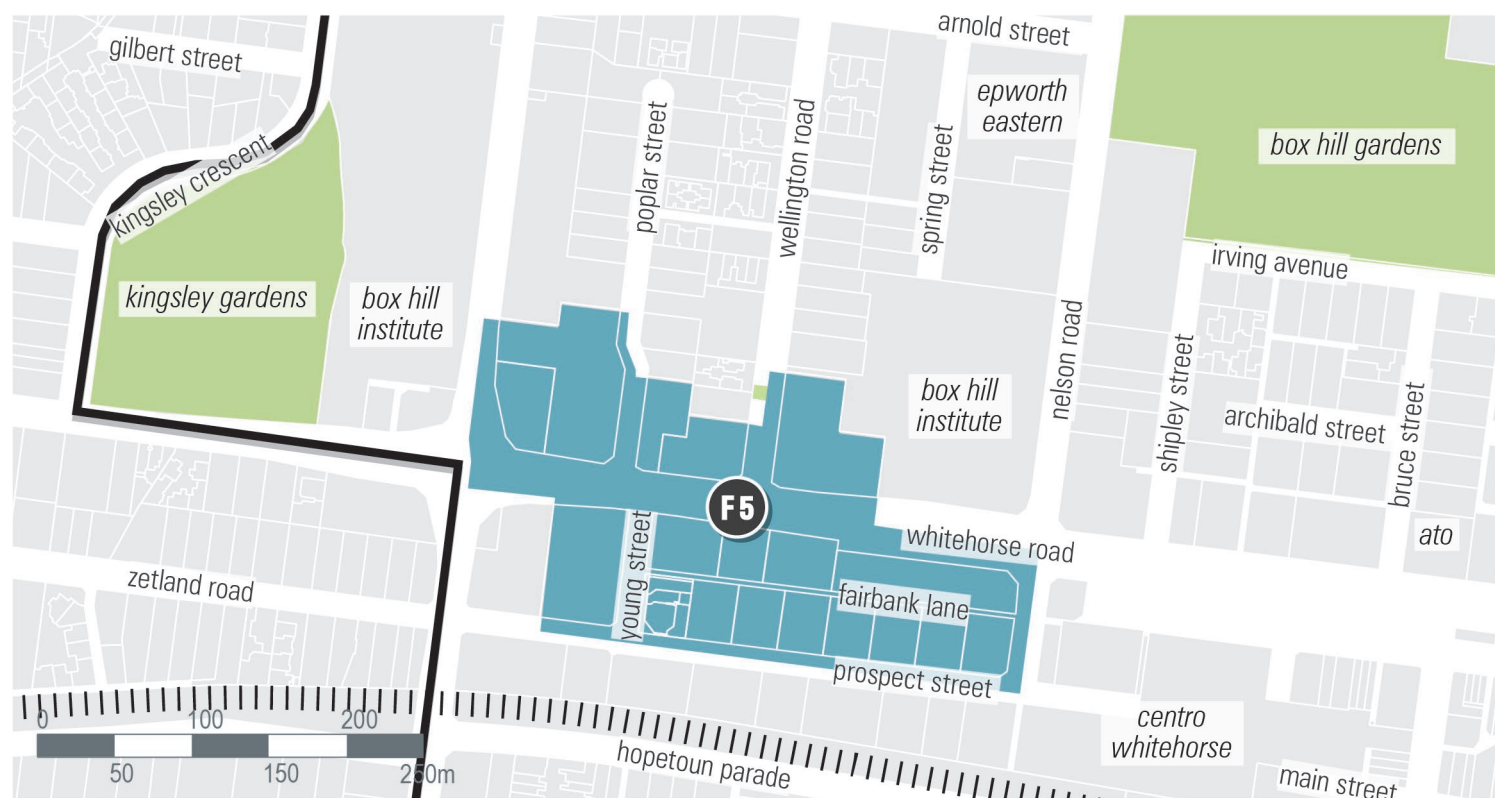


UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>Typical allotment size: Large (up to 2,500sqm) and Extra Large (greater than 2,500sqm)</li> <li>Typical allotment width: Extra Large (greater than 25m)</li> </ul>	<ul style="list-style-type: none"> <li>To establish a transition between Whitehorse Road West (Precinct F5) and the Traditional Town Centre (Precinct C/F1)</li> <li>To support high density mixed use development</li> <li>To facilitate a series of tall separated building forms on large and extra-large sites</li> <li>To encourage lot consolidation for medium and smaller sites</li> <li>To ensure sufficient separation between buildings to avoid excessive visual bulk</li> </ul>	<ul style="list-style-type: none"> <li>100% Site coverage for podium</li> <li>Refer to General Design Guidelines- Building Separation</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Whitehorse Road (60m)</li> <li>Prospect Street (15m)</li> <li>Market Street (pedestrian street)</li> </ul> </li> <li>Residential Interface:               <ul style="list-style-type: none"> <li>Hopetaun Parade</li> <li>Elgar Road</li> </ul> </li> <li>Recently constructed developments include the 20 storey ATO building (990 Whitehorse Road)</li> </ul>	<ul style="list-style-type: none"> <li>To establish a pedestrian scale urban environment</li> <li>To establish a consistent urban presentation along Whitehorse Road</li> <li>Retain a sense of openness along Whitehorse Road</li> <li>To establish a sense of transition on sites with a direct residential interface (outside the Activity Centre)</li> <li>To achieve activated ground level along its street interfaces</li> </ul>	<ul style="list-style-type: none"> <li>5 storey street wall to achieve a maximum 1:1 (street wall to street width) ratio</li> <li>Preferred setback of 5m above podium to all sides (minimum)</li> <li>Preferred maximum height of 20 storey</li> <li>A minimum 30 degrees angled setback profile above 15 storey for allotments with sensitive residential interface along Hopetaun Parade, Thurston Street and Elgar Road</li> <li>No setback from side and rear boundaries for up to 5 storeys</li> <li>Minimum 5m setback from side and rear boundaries for 6-20 storeys</li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>Hanbit Uniting Church (909 Whitehorse Road)- H090</li> </ul>	<ul style="list-style-type: none"> <li>To recognise the presence of an individual significant heritage building</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Key Views</b>	<ul style="list-style-type: none"> <li>Long range view to the Dandenong Ranges (east) along Whitehorse Road</li> </ul>	<ul style="list-style-type: none"> <li>To establish a sense of openness and retention of view corridor along Whitehorse Road to the Dandenong Ranges to the east</li> </ul>	<ul style="list-style-type: none"> <li>Refer to 'Street Walls &amp; Preferred Maximum Building Heights'</li> </ul>
<b>Additional street/ laneway address</b>	<ul style="list-style-type: none"> <li>All sites have access to multiple street frontages, or side/ rear laneways</li> <li>Laneways are generally consistently 3-3.5m wide</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Street Frontages</li> </ul>	<ul style="list-style-type: none"> <li>Setback ground level and level 1 from rear boundary by 1.5m to facilitate vehicular/ service access from rear laneways as required</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>Key Open Spaces:               <ul style="list-style-type: none"> <li>The central median of Whitehorse Road</li> <li>Market Street (north/ south)</li> </ul> </li> <li>Residential interface:               <ul style="list-style-type: none"> <li>Neighbourhood Residential Zone precinct west of Elgar Road</li> <li>Peripheral residential precinct along Hopetaun Parade</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>No landscaping provision has been accommodated within private properties</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives- Landscaping</li> </ul>	<ul style="list-style-type: none"> <li>Encourage establishment of green walls, or landscape elements within the building facade</li> <li>Incorporate landscaped gardens on podium roof top</li> </ul>

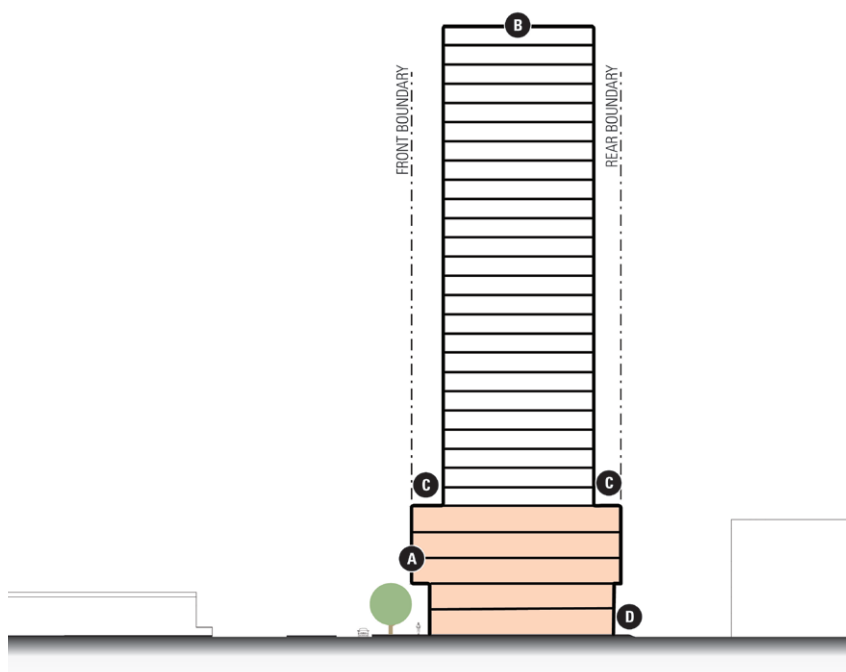
# Building Envelope Guidelines

## Precinct F5: Whitehorse Road West

F5



- A** 5 storey podium
- B** preferred max. building height 30 storey
- C** 5m setback above podium
- D** 1.5m setback from laneway for vehicular access as required



# Building Envelope Guidelines

## Precinct F5: Whitehorse Road West

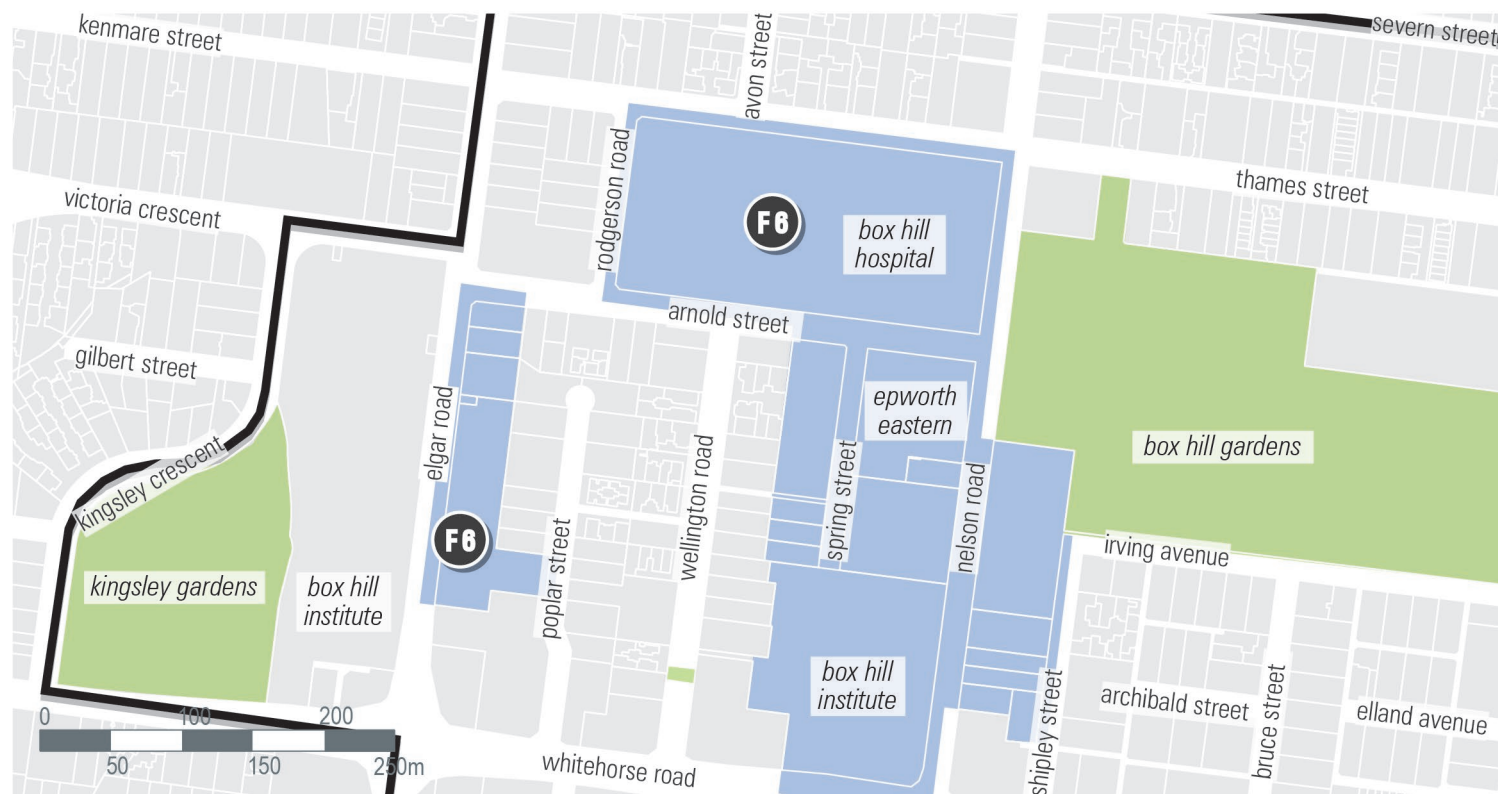


UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>Typical allotment size: Large (up to 2,500sqm) and Extra Large (greater than 2,500sqm)</li> <li>Typical allotment width: Extra Large (greater than 25m)</li> </ul>	<ul style="list-style-type: none"> <li>To support high density mixed use development</li> <li>To facilitate a series of tall separated building forms on large and extra-large sites</li> <li>To encourage lot consolidation for medium and smaller sites</li> <li>Ensure sufficient separation between buildings to avoid excessive visual bulk</li> </ul>	<ul style="list-style-type: none"> <li>100% site coverage at the ground level</li> <li>Refer to General Design Guidelines- Building Separation</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Whitehorse Road (30m)</li> <li>Prospect Street (15m)</li> </ul> </li> <li>Residential Interface:               <ul style="list-style-type: none"> <li>Elgar Road (west)</li> </ul> </li> <li>Approval for 30+ storey development at 850 Whitehorse Road</li> </ul>	<ul style="list-style-type: none"> <li>To establish a distinctive sense of arrival into Box Hill Activity Centre from the west</li> <li>To establish a pedestrian scale urban environment</li> <li>To establish a sense of transition on sites with a direct residential interface (outside the Activity Centre)</li> <li>To activate street interfaces at ground levels</li> </ul>	<ul style="list-style-type: none"> <li>5 storey street wall to achieve a maximum 1:1 (street wall to street width) ratio</li> <li>Preferred maximum height of 30 storey</li> <li>No setback from side and rear boundaries for up to 5 storey</li> <li>Preferred minimum setback of 5-8m above the podium to all sides (minimum):               <ul style="list-style-type: none"> <li>Minimum 5m setback from side and rear boundaries for 6-20 storey</li> <li>Minimum 8m setback from side and rear boundaries for 21-30 storey</li> </ul> </li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Key Views</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Additional street/ laneway address</b>	<ul style="list-style-type: none"> <li>All sites have access to multiple street frontages, or side/ rear laneways</li> <li>Laneways are generally consistently 3- 3.5m wide</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Street Frontages</li> <li>Additional pedestrian connection between Elgar and Wellington Road consistent to those identified within the Structure Plan</li> <li>To ensure priority pedestrian link is provided with active frontages</li> </ul>	<ul style="list-style-type: none"> <li>Setback ground level and level 1 from rear boundary by 1.5m to facilitate vehicular/ service access from rear laneways as required</li> <li>Align key view line with priority pedestrian link</li> <li>At grade pedestrian link that is open to the sky</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>Residential interface:               <ul style="list-style-type: none"> <li>Neighbourhood Residential Zone precinct west of Elgar Road</li> <li>Peripheral residential precinct along Hopetoun Parade</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>No landscaping provision has been accommodated within private properties</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives- Landscaping</li> <li>Improve pedestrian amenity along Elgar Road</li> </ul>	<ul style="list-style-type: none"> <li>Landscape setback along Elgar Road (minimum 5m) at the ground level</li> <li>Incorporate landscaped gardens on podium roof top</li> <li>Incorporate landscaping elements within the building façades where possible</li> </ul>

# Building Envelope Guidelines

## Precinct F6: TAFE & Hospital

F6



No typical cross section is applicable to Precinct F6

# Building Envelope Guidelines

## Precinct F6: TAFE & Hospital



F6

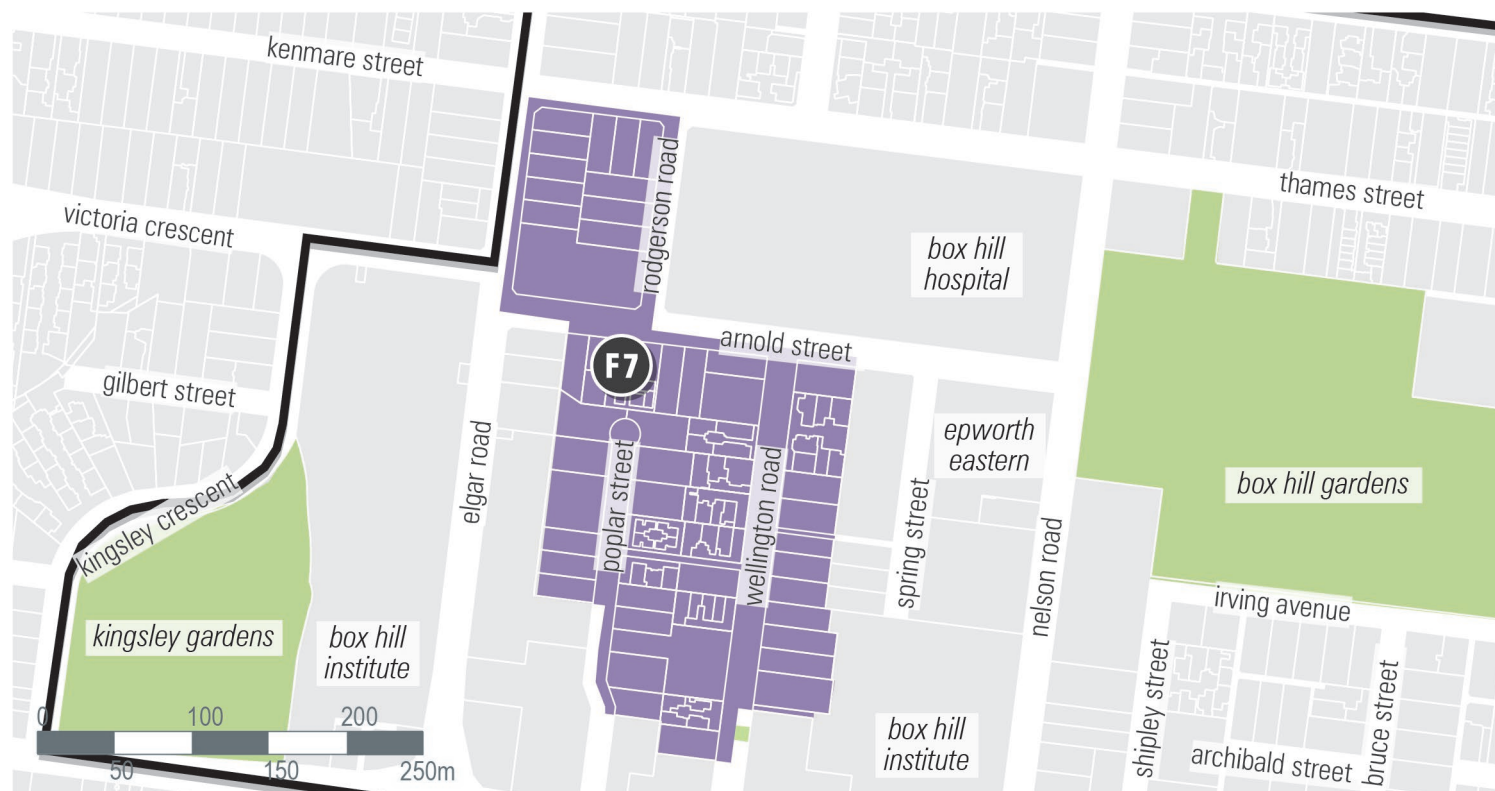
UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>Typical allotment size: Large (up to 2,500sqm) and Extra Large (greater than 2,500sqm)</li> <li>Typical allotment width: Extra Large (greater than 25m)</li> </ul>	<ul style="list-style-type: none"> <li>To support high density education/ institutional development within a generous landscape setting at the ground level (campus style)</li> <li>To encourage lot consolidation for medium and smaller sites</li> <li>To encourage taller forms with smaller footprints with a generous separation between buildings</li> </ul>	<ul style="list-style-type: none"> <li>60% site coverage</li> <li>A minimum 10m separation between buildings</li> <li>A plot - ratio approach is applicable on extra large sites</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Whitehorse Road (30m)</li> <li>Elgar Road (20m)</li> <li>Nelson Road (20m)</li> <li>Spring Street (15m)</li> <li>Arnold Street (20m)</li> </ul> </li> <li>Existing buildings are generally between 2-8 storey</li> </ul>	<ul style="list-style-type: none"> <li>To establish continuous belt of landscaping along all street frontages</li> <li>To encourage diversity of building types</li> <li>To ensure building orientation considers future development on or adjacent to the site, including potential linkages to such development</li> </ul>	<ul style="list-style-type: none"> <li>Minimum 8m landscape setback from all street frontages.</li> <li>Preferred maximum height of 15 storey</li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Key Views</b>	<ul style="list-style-type: none"> <li>Precinct F6 is located in between Box Hill Gardens and Kingsley Gardens with a provision for pedestrian connection between Poplar Street and Nelson Road</li> </ul>	<ul style="list-style-type: none"> <li>To ensure building orientation provides a positive relationship to the campus open space network and usable open space</li> <li>To ensure buildings 'frame' key viewlines</li> </ul>	<ul style="list-style-type: none"> <li>A minimum 10m separation between buildings</li> <li>Align key view line with priority pedestrian link</li> </ul>
<b>Additional street/ laneway address</b>	<ul style="list-style-type: none"> <li>All sites have access to multiple street frontages, or side/ rear laneways</li> </ul>	<ul style="list-style-type: none"> <li>Additional site permeability consistent to those identified within the Structure Plan</li> </ul>	<ul style="list-style-type: none"> <li>At grade pedestrian link that is open to the sky</li> <li>Encourage active frontages along pedestrian priority link</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>Existing buildings are generally low to medium rise with sufficient spacing in between buildings to facilitate solar penetrations at the lower levels</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>Campus style developments comprising permeable site planning and low to medium rise development set in an open car park, or landscape setting</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives- Landscaping</li> </ul>	<ul style="list-style-type: none"> <li>Landscape setback to all street edges (minimum 8m)</li> <li>Incorporate landscaping elements within the building façades where possible</li> <li>Incorporate public spaces at the ground level where possible</li> </ul>



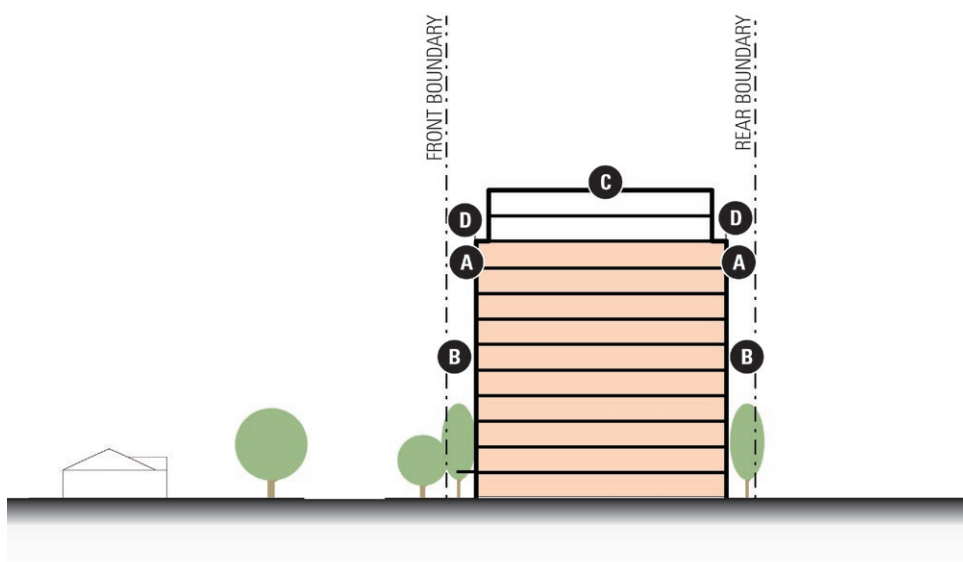
# Building Envelope Guidelines

## Precinct F7: Garden Infill

F7



- A** 10 storey street wall
- B** 5m setback from the street frontage and rear boundary
- C** preferred max. building height 12 storey
- D** 3m setback above street wall



# Building Envelope Guidelines

## Precinct F7: Garden Infill

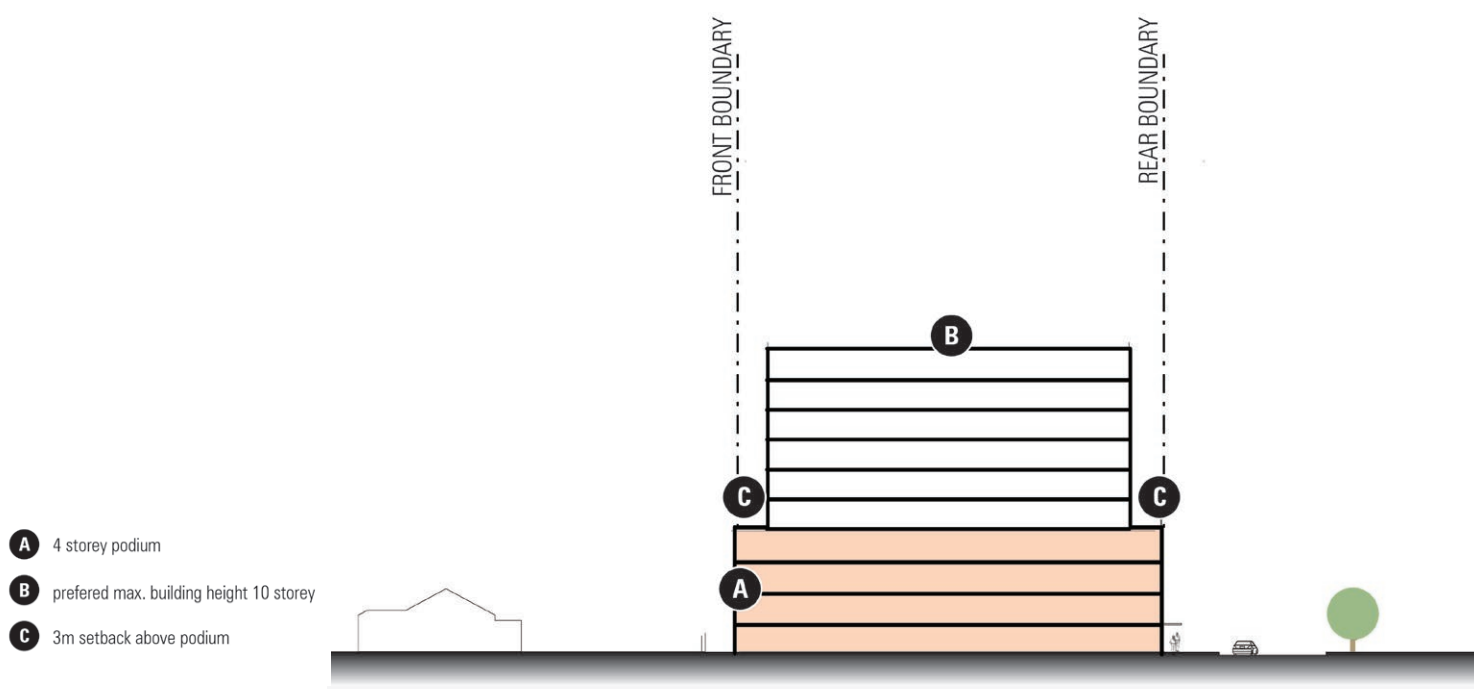


UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>Typical allotment size: Small (up to 1,000sqm) to Medium (up to 1,500sqm)</li> <li>Typical allotment width: medium to large (15-20m)</li> </ul>	<ul style="list-style-type: none"> <li>To support medium to high density development in a garden setting</li> </ul>	<ul style="list-style-type: none"> <li>80% site coverage</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Wellington Road (20m)</li> <li>Arnold Street (20m)</li> <li>Elgar Road (20m)</li> <li>Rodgers Road (15m)</li> <li>Poplar Street (15m)</li> </ul> </li> <li>Precinct F7 is surrounded by 'campus' precinct where buildings within established garden setting are encouraged</li> <li>Recent development approvals between 5 to 9 storey with varied street wall heights along Wellington Road and Poplar Street</li> </ul>	<ul style="list-style-type: none"> <li>To establish a pedestrian scale urban environment</li> <li>To retain a sense of openness with consistent front garden presentation along street frontages</li> <li>To secure long term future amenity on site</li> </ul>	<ul style="list-style-type: none"> <li>10 storey building base to achieve a maximum 1:1 (street wall to street width) ratio</li> <li>Preferred ground level setback of 5m from the street frontage (landscape zone)</li> <li>Preferred ground level setback of 5m from the rear boundary</li> <li>Preferred maximum height of 12 storey</li> <li>Preferred setback of 3m above the street wall</li> <li>Encourage party wall construction for up to 10 storey</li> <li>Minimum 5m setback from side and rear boundaries for 10-12 storey</li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Key Views</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Additional street/laneway address</b>	<ul style="list-style-type: none"> <li>Typically sites have single street frontage with no access to rear laneway</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Street Frontages</li> </ul>	<ul style="list-style-type: none"> <li>Accommodate a minimum 5m setback to the rear to achieve a consistent 10m separation between forms (up to 10 storey)</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>Allotments are generally orientated east - west resulting in greater overshadowing impact to the south</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>Existing garden character established by nature verges and street landscaping and open front gardens associated with existing dwellings, or more recent infill development</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives- Landscaping</li> </ul>	<ul style="list-style-type: none"> <li>Preferred ground level setback of 5m from the street frontage (landscape zone)</li> <li>Accommodate a minimum 5m setback to the rear to achieve a consistent 10m separation between forms (up to 10 storey)</li> </ul>

# Building Envelope Guidelines

## Precinct F8: Box Hill Gardens

**F8**



# Building Envelope Guidelines

## Precinct F8: Box Hill Gardens

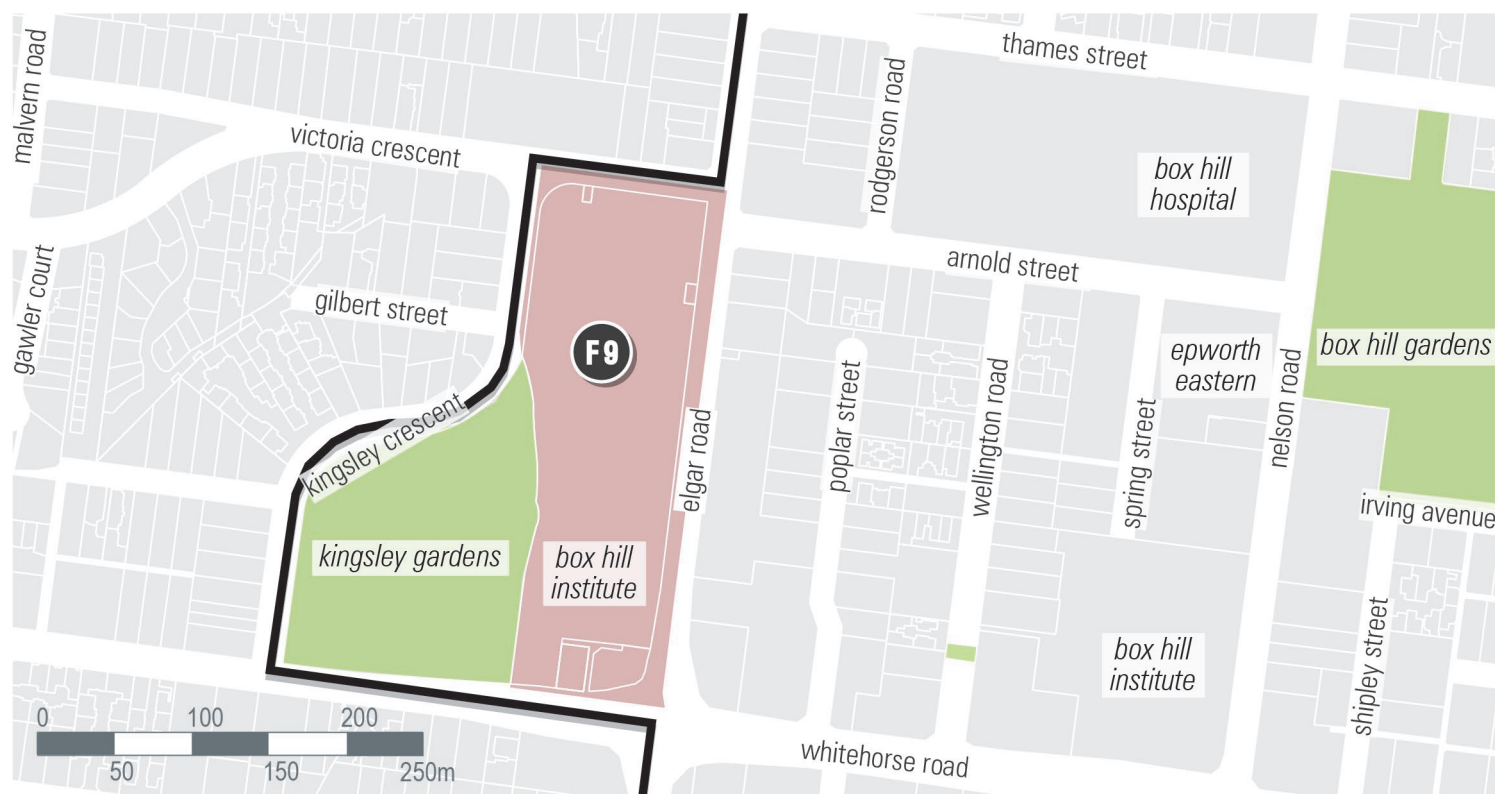

**F8**

UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>Typical allotment size: Small (up to 1,000sqm)</li> <li>Typical allotment width: Medium (15-20m)</li> </ul>	<ul style="list-style-type: none"> <li>To support medium density development</li> <li>To encourage lot consolidation to support development potential and minimise offsite impacts</li> </ul>	<ul style="list-style-type: none"> <li>100% site coverage at the ground level</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Irving Avenue (15m)</li> <li>Archibald Street (15m)</li> <li>Bruce Street (15m)</li> <li>Elland Avenue (15m)</li> <li>Shipley Street (15m)</li> </ul> </li> <li>Recent infill development of up to 9-10 storey with 4 storey street wall (constructed and approved)</li> </ul>	<ul style="list-style-type: none"> <li>To establish a pedestrian scale urban environment</li> <li>To retain a sense of openness with local streets</li> <li>To secure long terms future amenity on site</li> </ul>	<ul style="list-style-type: none"> <li>A 4 storey street wall to achieve a maximum 1:1 (street wall to street width) ratio</li> <li>Preferred maximum height of 10 storey</li> <li>Preferred setback of 3m above the street wall from the street frontage</li> <li>A minimum 5m setback to the side/rear above the street wall</li> <li>No setback from side and rear boundaries for up to 5 storey</li> <li>Minimum 5m setback from side and rear boundaries for 6-10 storey</li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Key Views</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Additional street/laneway address</b>	<ul style="list-style-type: none"> <li>All sites have access to multiple street frontages, or side/ rear laneways</li> </ul>	<ul style="list-style-type: none"> <li>To maximise opportunities for outlook onto the public realm</li> <li>Ensure future amenity is provided on site</li> </ul>	<ul style="list-style-type: none"> <li>Communal open space should be provided within each development</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>Allotments are generally orientated east - west resulting in greater overshadowing impact to the south</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>Bruce Street is a local street with landscaping provisions currently accommodated within private land</li> </ul>	<ul style="list-style-type: none"> <li>To encourage positive landscape contribution onto the public realm</li> </ul>	<ul style="list-style-type: none"> <li>Incorporate landscaping elements within the building façades where possible</li> </ul>

# Building Envelope Guidelines

## Precinct F9: Kingsley Gardens

**F9**



No typical cross section is applicable to Precinct F9

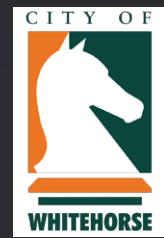


# Building Envelope Guidelines

## Precinct F9: Kingsley Gardens


**F9**

UD Attributes	Existing context	Precinct Objectives	Built Form Response
<b>Subdivision pattern</b>	<ul style="list-style-type: none"> <li>The precinct is dominated by a single extra large parcel (greater than 2,500sqm), currently occupied by the Box Hill Institute.</li> </ul>	<ul style="list-style-type: none"> <li>To support medium density civic/ institutional development within a generous landscape setting at the ground level (campus style)</li> <li>To encourage lot consolidation for medium and smaller sites</li> <li>To encourage taller forms with smaller footprints with a generous separation between buildings</li> </ul>	<ul style="list-style-type: none"> <li>60% site coverage</li> <li>A plot - ratio approach is applicable on extra large sites</li> </ul>
<b>Street walls &amp; preferred maximum heights</b>	<ul style="list-style-type: none"> <li>Primary frontage:               <ul style="list-style-type: none"> <li>Whitehorse Road (30m)</li> <li>Arnold Street (20m)</li> <li>Elgar Road (20m)</li> </ul> </li> <li>Residential interface:               <ul style="list-style-type: none"> <li>Victoria Crescent</li> <li>Kingsley Crescent</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>To establish continuous belt of landscaping along all street frontages</li> <li>To ensure building orientation considers future development on or adjacent to the site, including potential linkages to such development</li> </ul>	<ul style="list-style-type: none"> <li>Minimum 8m landscape setback from all street frontages.</li> <li>Preferred maximum height of 8 storey</li> </ul>
<b>Heritage</b>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>N/A</li> </ul>
<b>Key Views</b>	<ul style="list-style-type: none"> <li>Precinct F9 is located in east of Kingsley Gardens with a provision for pedestrian connection between Kingsley Gardens and Elgar Road</li> </ul>	<ul style="list-style-type: none"> <li>To ensure building orientation provides a positive relationship to the campus open space network and usable open space</li> <li>To ensure buildings 'frame' key viewlines</li> </ul>	<ul style="list-style-type: none"> <li>A minimum 10m separation between buildings</li> <li>Align key view line with priority pedestrian link</li> </ul>
<b>Additional street/ laneway address</b>	<ul style="list-style-type: none"> <li>All sites have access to multiple street frontages, or side/ rear laneways</li> </ul>	<ul style="list-style-type: none"> <li>To improve pedestrian permeability</li> <li>To maximise opportunities for outlook onto the public realm</li> </ul>	<ul style="list-style-type: none"> <li>Encourage active frontages along Kingsley Gardens at the ground level</li> <li>Provide ground level east - west connection at regular intervals between Elgar Road and Kingsley Gardens</li> <li>At grade pedestrian link that is open to the sky</li> </ul>
<b>Amenity/ access to daylight</b>	<ul style="list-style-type: none"> <li>Existing buildings are generally low to medium rise with sufficient spacing in between buildings to facilitate solar penetrations at the lower levels</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives - Overshadowing</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Guidelines - Overshadowing</li> </ul>
<b>Landscape</b>	<ul style="list-style-type: none"> <li>Campus style developments comprising permeable site planning and low to medium rise development set in an open car park, or landscape setting.</li> </ul>	<ul style="list-style-type: none"> <li>Refer to General Design Objectives- Landscaping</li> </ul>	<ul style="list-style-type: none"> <li>Landscape setback to all street edges (minimum 5m)</li> <li>Incorporate landscaping elements within the building façades where possible</li> <li>Incorporate public spaces at the ground level where possible</li> </ul>



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